

The Hongkong Telegraph.

(ESTABLISHED 1881.)

NEW SERIES No 4875

戊午年五月十五日

SATURDAY, JUNE 17, 1905.

大英報

香港大英報

\$30 PER ANNUM.
SINGLE COPY, 10 CENTS.

Banks.

YOKOHAMA SPECIE BANK, LIMITED.
ESTABLISHED 1880.
CAPITAL SUBSCRIBED..... Yen 24,000,000
CAPITAL PAID UP..... " 18,000,000
CAPITAL UNCALLED..... " 6,000,000
RESERVE FUND..... " 9,720,000
Head Office—YOKOHAMA.

Branches and Agencies:
TOKIO. NEW YORK.
NAGASAKI. HONOLULU.
LYONS. SHANGHAI.
SAN FRANCISCO. NEWCHOWWANG.
BOMBAY. MUKDEN.
TIENTIN. PORT ARTHUR.
PEKING. CHEFOO.
KOBE. DALNY.
LONDON.

LONDON BANKERS:
THE LONDON JOINT STOCK BANK, LTD.
PARKS' BANK, ED.
THE UNION OF LONDON AND
SMITH'S BANK, LTD.
HONGKONG BRANCH—INTEREST ALLOWED:
On Current Account at the rate of 2 per cent.
On Fixed Deposits for 12 months at 5 per cent.
" " "

TAKEO TAKAMICHI,
Manager.
Hongkong, 23rd May, 1905. [20]

**HONGKONG AND SHANGHAI
BANKING CORPORATION.**
PAID-UP CAPITAL..... \$10,000,000
RESERVE FUND—
Sterling Reserve..... \$10,000,000
Silver Reserve..... \$ 8,000,000
RESERVE LIABILITY OF PROPRIETORS \$10,000,000

COURT OF DIRECTORS:
H. A. W. SLADE, Esq., Chairman;
A. HAUFF, Esq., Deputy Chairman;
Hon. C. W. Dickson, H. Schubart, Esq.
E. Goetz, Esq.
G. H. Medhurst, Esq.
A. J. Raymond, Esq.
F. Salinger, Esq.

CHIEF MANAGER:
Hongkong—J. R. M. SMITH,
MANAGER;
Shanghai—H. E. R. HUNTER.
LONDON BANKERS—LONDON AND COUNTY
BANKING COMPANY, LIMITED.

HONGKONG—INTEREST ALLOWED:

On Current Account at the rate of 2 per cent.
per Annum on the daily balance.

ON FIXED DEPOSITS:
For 3 months, 2½ per cent. per Annum.
For 6 months, 3 per cent. per Annum.
For 12 months, 4 per cent. per Annum.

J. R. M. SMITH,
Chief Manager.

Hongkong, 17th May, 1905. [21]

HONGKONG SAVINGS BANK.

THE Business of the above Bank is conducted
by the HONGKONG AND SHANGHAI
BANKING CORPORATION. Rules may be
obtained on application.

INTEREST on deposits is allowed at 3½ per cent. per annum.

Depositors may transfer at their option
balances of \$100 or more to the HONGKONG AND
SHANGHAI BANK to be placed on FIXE
DEPOSIT at a PER CENT. per annum.

For the HONGKONG AND SHANGHAI
BANKING CORPORATION,
J. R. M. SMITH,
Chief Manager.

Hongkong, 1st May, 1905. [22]

**THE
DEUTSCH ASIATISCHE BANK**

AUTHORIZED CAPITAL..... Sh. Taels 7,500,000

HEAD OFFICE—SHANGHAI.
BOARD OF DIRECTORS: BERLIN.

BRANCHES:
Berlin Calcutta Hankow
Tientsin Tsingtao Tsinanfu

LONDON BANKERS:

Messrs. N. M. ROTHSCHILD & SONS,

UNION OF LONDON AND SMITH'S BANK, LTD.

DEUTSCHE BANK (BERLIN), LONDON AGENCY

DIRECTION DER DISCONTO GESELLSCHAFT.

INTEREST allowed on Current Account.

DEPOSITS received on terms which may be learned on application. Every description of Banking and Exchange business transacted.

M. HOMANN,
Manager.

Hongkong, 1st April, 1905. [23]

Insurance.

NORTH GERMAN FIRE INSURANCE
COMPANY OF HAMBURG.

THE Undersigned AGENTS of the above Company are prepared to accept 1st Class FOREIGN and CHINESE RISKS & CURRENT RATES.

SIEMSEN & Co.

Hongkong, 28th May, 1905. [24]

JAPAN



COALS.

**THE MITSUI BUSSAN KAISHA
(MITSUI & Co.)**

HEAD OFFICE—1, SURUGA-CHO, TOKYO.
LONDON BRANCH—14, LIME STREET, E.C.
HONGKONG BRANCH—PRINCE'S BUILDINGS, ICE HOUSE STREET.

OTHER OFFICES:
New York, San Francisco, Hamburg, Bombay, Singapore, Sourabaya, Manila, Amoy, Shanghai, Chefoo, Tientsin, Newchowwang, Port Arthur, Seoul, Chempoo, Yokohama, Yokosuka, Nagoya, Osaka, Kobe, Madras, Kure, Shimoneseki, Moji, Wakamatsu, Karatsu, Nagasaki, Kuchinotsu, Sasebo, Mikaze, Hakodate, Taipeh, &c.

Telegraphic Address: "MITSUI" (A.B.C. and A1 Codes).
CONTRACTORS OF COAL to the Imperial Japanese Navy and Arsenals and the State Railways; Principal Railway Companies and Industrial Works; Home and Foreign Mail and Freight Steamers.

SOLE PROPRIETORS of the Famous Miike, Tagawa, Yamana and Ida Coal Mines; and SOLE AGENTS for Fujianota, Hokku, Hondo, Ichinura, Kanada, Mamoda, Manoura, Oocra, Otoej, Saishara, Tsubakuro, Yoshinotani, Yoshio, Yanakibara and other Coals.

S. MINAMI, Manager, Hongkong. [25]

PENINSULAR AND ORIENTAL STEAM NAVIGATION COMPANY.

FOR	STEAMERS	TO SAIL ON	REMARKS.
YOKOHAMA VIA SHANGHAI, MOJI and KOBE.	FORMOSA.....	About 24th June	Freight and Passage. (Passing through the Inland Sea.)
THREE-NEEDLE HOUSE, E.C.	B. W. H. Snow.....	June	Passage.
LONDON BANKERS: NATIONAL PROVINCIAL BANK OF ENGLAND, LIMITED, UNION OF LONDON AND SMITH'S BANK, LTD. BRITISH LINEN COMPANY BANK.	PALERNO.....	About 27th June	Freight only.
BRANCHES AND AGENTS ALL OVER THE WORLD.	COLOMBO, FORT SAID and MARSELLES.....	June	
THE Corporation transacts every Description of Banking and Exchange Business, receives Money in Current Account and accepts Fixed Deposits at Rates which may be ascertained on application.	CHUSAN.....	About 30th June	Freight and Passage.
CHARLES R. SCOTT, Manager.	E. G. Andrew.....	July 1st, Noon	See Special Advertisement.
20, Des Voeux Road, Hongkong, 26th May, 1905. [21]	NUBIA.....	July 1st, Noon	

For Further Particulars, apply to

L. S. LEWIS, Acting Superintendent.

Hongkong, 17th June, 1905. [22]

Intimations.

LANE, CRAWFORD & CO.



4 or 5 or 10 Catty Boxes constitutes one of the most acceptable
Presents to those at Home.

Without doubt this is the Finest Blend of TEA at the
Price, to be had in China.

1905
CUMSHAW TEA

LANE, CRAWFORD & CO.

ARE NOW BOOKING ORDERS FOR THE ABOVE SPECIALLY BLENDED
FOOCHOW TEA.

PRICES:
Including Freight, Duty and Delivery to any address in the United Kingdom,
Per 10 Catty Box, \$17.50. Per 5 Catty Box, \$10.00.
Hongkong, June 16th, 1905. [24]

KÜPPER'S PILSENER BEER.

The best PILSENER in the East; ask for Küpper,
and see that you get it.

CALDBECK, MACGREGOR & CO.,
SOLE AGENTS.

15, QUEEN'S ROAD CENTRAL,
Hongkong, 6th June, 1905. [25]

E. C. WILKS & CO., CONSULTING ENGINEERS, ETC.

are Sole Agents for—

The General Electric Co. of London, and have a large stock of Electric Fittings, Cables, Wire, Lamps, Electroliers, etc., on hand,

AT 12, BEACONSFIELD ARCADE,

HONGKONG.

INSPECTION INVITED.

ESTIMATES GIVEN.

Also Sole Agents for:

H. W. John's Canadian Asbestos Goods, stock on hand.

THORNYCROFT MOTORS.

W. H. ALLEN & SON, BEDFORD.

Stewart & Lloyd's Steel Plates, Angles,

Tubes, &c.

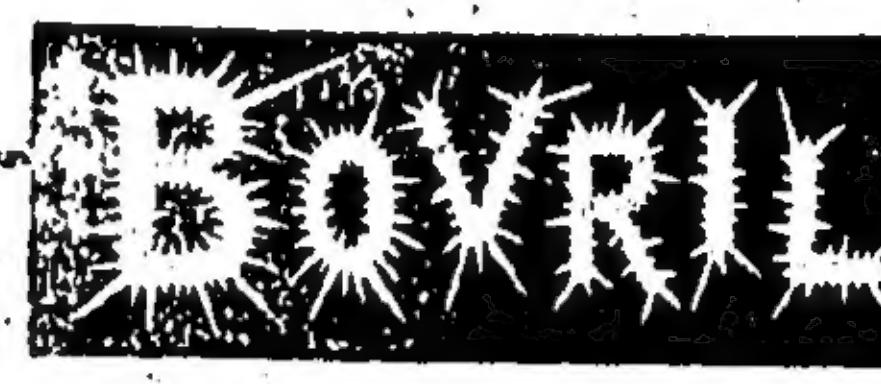
Hongkong, 1st June, 1905. [26]

Intimations.

If it's power
you want,

take

BOVRIL



PURE AND CHEAP

California Riesling	- - -	\$6.50 per Dozen Quarts
Do.	- - -	3.75 " " Pints
Do.	- - -	6.50 " " Quarts
Do.	- - -	3.75 " " Pints

H. PRICE & CO.,
12, QUEEN'S ROAD.

Hongkong, 19th May, 1905. [26]

PHOTO SUPPLIES.

LONG, HING & CO.,

17, QUEEN'S ROAD.

Hongkong, 15th May, 1905. [27]

Hotels.

HONGKONG HOTEL.

Military Band during dinner on Saturday Nights.

A. F. DAVIES,
Acting Manager.

Hongkong, 7th February, 1905. [28]

VICTORIA HOTEL,

SHAMEEN, CANTON,

ON THE BRITISH CONCESSION.

MACAO HOTEL,

MACAO, CHINA,

IN THE CENTRE OF THE PRAIA GRANDE.

BOTH HOTELS UNDER EXPERIENCED EUROPEAN MANAGEMENT.

EVERY COMFORT AND CONVENIENCE FOR RESIDENTS AND TOURISTS.

WM. FARMER, Proprietor.

17, QUEEN'S ROAD CENTRAL.

HONGKONG HIGH-LEVEL TRAMWAYS COMPANY, LIMITED.

TIME TABLE.

WEEK DAYS.

7.00 a.m. to 7.30 a.m. ... Every 30 minutes.

7.30 a.m. to 8.00 a.m. ... Every 10 minutes.

8.00 a.m. to 8.30 a.m. ... Every 15 minutes.

8.30 a.m. to 9.30 a.m. ... Every 10 minutes.

9.30 a.m. to 11.00 a.m. ... Every 15 minutes.</p

Shipping—Steamers.

HONGKONG, CANTON, MACAO AND
WEST RIVER STEAMERS.
JOINT SERVICE OF THE HONGKONG, CANTON AND MACAO
STEAMBOAT CO., LTD., AND THE CHINA NAVIGATION
COMPANY, LTD.

HONGKONG-CANTON LINE.

S.S. "HONAM,"	2,633 tons.....	Captain H. D. Jones.
" " FOWAN,"	4,330 "	G. F. Morrison, R.M.R.
" " FATSHAN,"	3,200 "	R. D. Thomas.
" " HANKOW,"	3,673 "	C. V. Lloyd.
" " KINSHAN,"	1,995 "	J. J. Louis.

Departures from HONGKONG to CANTON daily at 8.30 A.M. (Sunday excepted); 9 P.M. and 10.30 P.M. (Saturday excepted).

Departures from CANTON to HONGKONG daily at 8.30 A.M., 3 P.M. and 6 P.M. (Sunday excepted).

These Steamers, carrying His Majesty's Mail, are the largest and fastest on the River, Special attention is drawn to their Superior Saloon and Cabin accommodation.

SERVICE OF THE HONGKONG, CANTON AND MACAO STEAMBOAT CO., LTD.

HONGKONG-MACAO LINE.

S.S. "HEUNGSHAN," 1,993 tons..... Captain W. E. Clarke.
Departures from Hongkong to Macao on week days about 3 P.M. (See special Summer Time-table). Departures on Sundays at Noon.
Departures from Macao to Hongkong daily at 8 A.M.

CANTON-MACAO LINE.

S.S. "LUNGSHAN," 319 tons..... Captain T. Hamlin.
This steamer leaves Canton for Macao every Tuesday, Thursday and Saturday at about 8.30 A.M.; and leaves Macao for Canton every Monday, Wednesday and Friday at about 8 A.M.

JOINT SERVICE OF THE H.K., C. AND MACAO STEAMBOAT CO., LTD.
THE CHINA NAVIGATION COMPANY, LTD., AND THE INDO-CHINA STEAM NAVIGATION
COMPANY, LTD.

CANTON-WUCHOW LINE.

S.S. "SAINAM," 588 tons..... Captain W. A. Valentine.
" NANNING," 599 " Captain C. Butchart.
One of the above steamers leaves Canton for Wuchow every Monday, Wednesday and Friday at about 8.30 A.M. calling at Yunki, Mahning, Kun-chuk, Kau-kong, Samshui, Hui-wu, Shiu-hing, Luk-po, Luk-to, Lo-Ting-Hau, Tak-hing, Doshing and Fong-Chuen. Departures from Wuchow for Canton calling at the above ports every Monday, Wednesday and Friday at about 8.30 A.M.

FARES:—Canton to Wuchow Single \$15.00, Return \$25.00.
Canton to Tak-hing Single \$12.50, Return \$21.00.
Canton to Samshui Single \$7.50.

HONGKONG-WUCHOW LINE.

S.S. "LINTAN," Capt. B. Branch. S.S. "SANU," Capt. J. Wilcox.
Departures from Hongkong to Wuchow about three times every week, calling at Kun-chuk, Samshui, Shiu-hing and Tak-hing. Departures from Wuchow about three times every week calling at the same ports.

FARES:—Hongkong to Wuchow Single \$17.50, Return \$30.00. Round trip tickets to Wuchow returning via Canton or vice versa \$30.00, available for one month. Round trips to and from Wuchow take from 5 to 7 days.

HONGKONG-KONGMOON LINE.

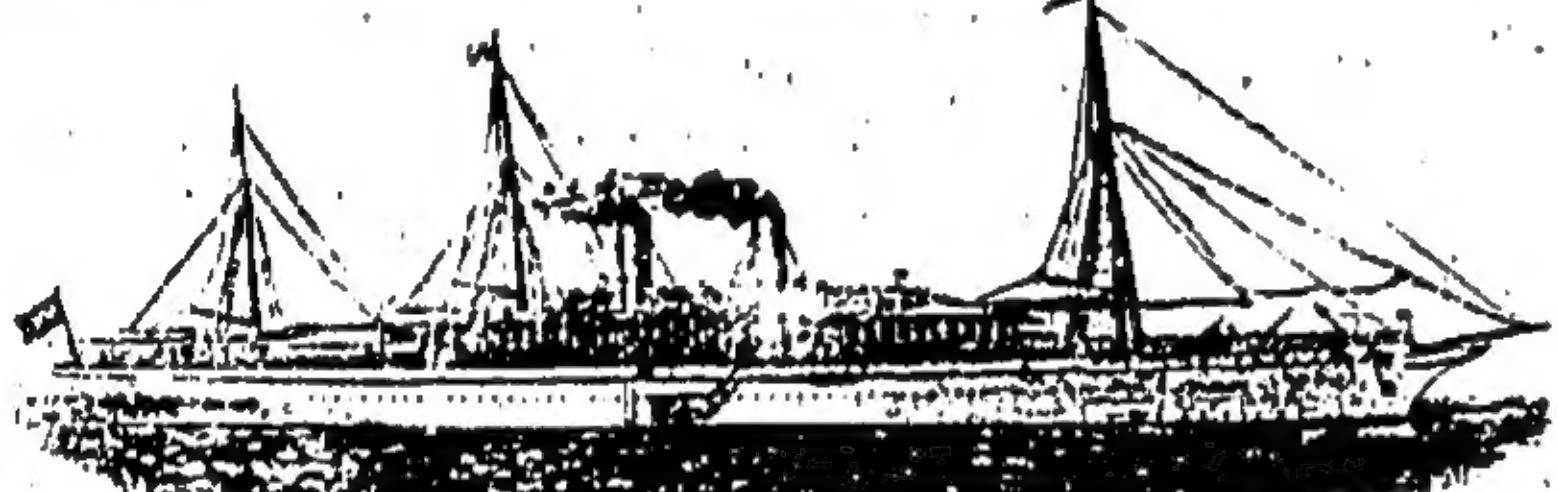
S.S. "TAK HING," Capt. R. Birns. S.S. "HONGKONG," Capt. Maxfield.
Departures from Hongkong daily (Saturday excepted) at 7 P.M., calling at Kun-chuk, Samshui, Shiu-hing and Tak-hing. Returning daily (Monday excepted).

FARES:—Hongkong to Kong Moon, Single \$6.00
Hongkong to Kun-chuk, Single \$7.00.

The above vessels have superior Saloon and Cabin accommodation and are lighted throughout by electricity. Meals charged extra.

Further particulars may be obtained at the Office of the—
HONGKONG, CANTON & MACAO STEAMBOAT CO., LTD.,
18, Bank Buildings, Queen's Road Central, opposite the Hongkong Hotel
Or of BUTTERFIELD & SWIRE,
Agents, CHINA NAVIGATION CO., LTD.

Hongkong, 17th June, 1905.

CANADIAN PACIFIC RAILWAY COY'S
ROYAL MAIL STEAMSHIP LINE.

THE FAST ROUTE BETWEEN CHINA, JAPAN AND EUROPE, VIA CANADA
AND THE UNITED STATES.
(CALLING AT SHANGHAI, NAGASAKI, KOBE, YOKOHAMA & VICTORIA, B.C.)
SAVING 3 TO 7 DAYS ACROSS THE PACIFIC.—(SUBJECT TO ALTERATION).

PROPOSED SAILINGS FROM HONGKONG.—(SUBJECT TO ALTERATION).

Steamers. Tons. Commanders. Sailing Dates.
R.M.S. "EMPEROR OF INDIA," 6,000. E. Beetham, R.M.R. WEDNESDAY, 21st June.
" " "TARTAR," 4,425. W. Davison, R.M.R. WEDNESDAY, 5th July.
" " "EMPEROR OF JAPAN," 6,000. H. Pybus, R.M.R. WEDNESDAY, 12th July.
" " "EMPEROR OF CHINA," 6,000. R. Archibald, R.M.R. WEDNESDAY, 2nd Aug.
" " "ATHENIAN," 3,440. S. Robinson, R.M.R. WEDNESDAY, 9th Aug.
Hongkong to London, 1st Class, via St. Lawrence f/60. Vid New York £6.
Hongkong to London, Intermediate on
Steamers, and 1st Class Rail £4.00. " £4.00.

THE magnificent Twin-screw "EMPEROR" Steamships pass through the famous INLAND SEA OF JAPAN, and usually make the voyage YOKOHAMA TO VANCOUVER (B.C.) in 12 DAYS, and make connection with the PALATIAL OVERLAND TRAINS FROM THE PACIFIC TO THE ATLANTIC WITHOUT CHANGE.

R.M.S. "TARTAR" and "ATHENIAN" carry "Intermediate" Passengers only at intermediate rates, affording superior accommodation for that class.

Passengers Booked through to all principal points and AROUND THE WORLD.
SPECIAL RATES (First class only) granted to Missionaries, Members of the Naval, Military, Diplomatic and Civil Services, and to European Officials in the Service of Chinese and Japanese Governments.

For further information Maps, Guides, Hand Books, Rates of Freight and Passage, apply to

Hongkong, 14th June, 1905.

HAMBURG-AMERIKA LINIE.
OESTERASIATISCHER DIENST.

(Taking cargo at through Rates to ANTWERP, AMSTERDAM, ROTTERDAM, COPENHAGEN, LISBON, OPORTO, LONDON, LIVERPOOL, GLASGOW, TRIESTE, GENOA, PORTS IN THE LEVANTE; BLACK SEA and BALTIC PORTS; NORTH and SOUTH AMERICAN PORTS).

PROPOSED SAILINGS FROM HONGKONG
SUBJECT TO ALTERATION.

STEAMERS. DESTINATIONS. SAILING DATES.
G. FERD. LAEISZ: HAVRE, DUNKIRK, B'MEN & H'BURG. } 22nd June. } Freight.
von Hoff } (Calling at S'PORE, PENANG & COLOMBO). }
BRISGAVIA } HAVRE, ANTWERP and HAMBURG. } 30th June. } Freight.
Russ } (Calling at S'PORE, PENANG & COLOMBO). }
SITHONIA } HAVRE, BREMEN and HAMBURG. } 12th July. } Freight.
Hildebrandt } (Calling at S'PORE, PENANG & COLOMBO). }
ACILIA } HAVRE and HAMBURG. } 26th July. } Freight.
Schilke } (Calling at S'PORE, PENANG & COLOMBO). }
ALESIA } HAVRE and HAMBURG. } 10th August. } Freight.
Sachs } (Calling at S'PORE, PENANG & COLOMBO). }
For further Particulars, apply to

HAMBURG-AMERIKA LINIE,
HONGKONG OFFICE,
No. 4, Queen's Building.

Hongkong, 15th June, 1905.

D. NOMA, TATTOOER,
6, QUEEN'S ROAD CENTRAL.

THE Public are informed that my Parlours are open from 9 A.M. all day. My 32 years' experience in TATTOOING is a guarantee of good work and prompt execution. My Colours are absolutely fast and perfectly harmless, and produce a charming effect not attained by any other, as their composition is only known to me. H. R. H. The Duke of York, and H. I. H. The Emperor of Russia, both honoured me with their patronage; besides many others of High Rank. Prices Moderate and satisfaction guaranteed as attested by 3,700 Recommendations which I have received from all sources.

Hongkong, 16th November, 1904.

10

Advertisers.

IMPERIAL GERMAN MAIL LINES.

NORDDEUTSCHE LLOYD, BREMEN.

EUROPEAN LINE.

STEAM FOR SINGAPORE, PENANG, COLOMBO, ADEN, SUET, PORT SAID, NAPLES, GENOA, ANTWERP, BREMEN/HAMBURG;

PORTS IN THE LEVANTE, BLACK SEA AND BALTIC PORTS;

ALSO

LONDON, NEW YORK, BOSTON, BALTIMORE, NEW ORLEANS, GALVESTON,
AND SOUTH AMERICAN PORTS;

Steamers will call at GIBRALTAR and SOUTHAMPTON to land Passengers
and Luggage.

H.E.—Cargo can be taken on through Bills of Lading for the Principal Places in Russia.

PROPOSED SAILINGS FROM HONGKONG.

(SUBJECT TO ALTERATION.)

STEAMERS.

SAILING DATES.

ZIETEN	WEDNESDAY, 21st June.
DARMSTADT	WEDNESDAY, 5th July.
SACHSEN	WEDNESDAY, 12th August.
SCHARNHORST	WEDNESDAY, 19th August.
PRINZ HEINRICH	WEDNESDAY, 26th August.
PRINZ EITEL FRIEDRICH	WEDNESDAY, 2nd September.
PREUSSEN	WEDNESDAY, 9th September.
ROON	WEDNESDAY, 16th September.
BAUERN	WEDNESDAY, 23rd September.
GNEISENAU	WEDNESDAY, 30th October.
PRINZESS ALICE	WEDNESDAY, 6th November.
SACHSEN	WEDNESDAY, 13th December.
PRINZ REGENT LUFTPOL	WEDNESDAY, 20th December.
PRINZ HEINRICH	

ON WEDNESDAY, the 21st day of June, 1905, at Noon, the Steamship "ZIETEN" of the NORDDEUTSCHE LLOYD, Captain F. von Hinter, with MAILED, PASSENGERS, SPECIE and CARGO, will leave this Port as above, Calling at NAPLES and GENOA.

Shipping Orders will be granted till Noon, on MONDAY, the 19th June, Cargo and Specie will be received on Board until 5 P.M., on TUESDAY, the 20th June, and Parcals will be received at the Agency's Office until NOON, on TUESDAY, the 20th June.

Contents of Packages required: No Parcel Receipts will be signed for less than \$2.50 and Parcals should not exceed Two Cubic Feet in Measurement.

The Steamer has splendid Accommodation and carries a Doctor and a Stewardess.

Linen can be washed on board.

JAPAN-CHINA-AUSTRALIA LINE,
VIA NEW GUINEA.

STEAM FOR FRIEDRICH-WILHELMSHAFEN, HERBERTSHOEHE,
MATUPI, BRISBANE, SYDNEY AND MELBOURNE.

PROPOSED SAILINGS FROM HONGKONG.

(Subject to alteration.)

STEAMERS.

SAILING DATES.

WILLEHAD	TUESDAY, 27th June.
PRINZ WALDEMAR	TUESDAY, 25th July.
PRINZ SIGISMUND	TUESDAY, 22nd August.

ON TUESDAY, the 27th June, 1905, at Noon, the Steamship "WILLEHAD", Captain F. Ph. Obenauer, with Mailes, Passengers and Cargo, will leave this port as above. The steamer has splendid accommodation and carries a Doctor and a Stewardess.

Linen can be washed on board.

SAILINGS OUTWARDS.

EUROPEAN & AUSTRALIAN SERVICE.

FOR STEAMERS ABOUT

KOBE & YOKOHAMA PRINZ WALDEMAR TUESDAY, 20th June.

SHANGHAI, NAGASAKI, SACHSEN WEDNESDAY, 21st June.

SHANGHAI, NAGASAKI, SCHARNHORST WEDNESDAY, 5th July.

KOBE & YOKOHAMA SACHSEN WEDNESDAY, 12th July.

NORDDEUTSCHER LLOYD.

For further Particulars, apply to

MELCHERS & CO., AGENTS.

Hongkong, 7th June, 1905.

10

10

10

10

10

10

10

10

10

10

10

10

10

10

10

10

Intimations.

A. S. WATSON & CO.,
LIMITED.

ESTABLISHED 1841.

WINE AND SPIRIT
MERCHANTS.

ALEXANDRA BUILDINGS.

SCOTCH WHISKY.
WATSON'S
"GLENORCHY"
BLEND.

("B" Quality.)

PURE MALT WHISKY.
VERY OLD.

A FINE "SODA" WHISKY.

\$12.00.....Per Case

Equal to any imported Bottled Whisky at
\$16.00.

WATSON'S
CELEBRATED

E
BLEND

VERY OLD LIQUOR SCOTCH

WHISKY.

The Perfection of Whiskies.
\$16.50.....Per Case.

A. S. WATSON & Co.,
LIMITED,

WINE AND SPIRIT MERCHANTS.
BLENDERS AND BOTTLERS.

ESTABLISHED 1841.

Hongkong, 17th June, 1905.

(32)

GREGOR & CO.,

34, QUEEN'S ROAD CENTRAL,

1ST FLOOR.

OUR OWN
BRANDS:

HOME BOTTLED:

GREGOR & Co.'s IMPERIAL

HIGHLAND WHISKY..... \$16.00

GREGOR & Co.'s CLUB No. 1

WHISKY 18.00

GREGOR & Co.'s ROYAL OLD

LIQUEUR WHISKY..... 24.00

GREGOR & Co.'s TARRAGONA 9.00

GREGOR & Co.'s OLD TAWNY

PORT 11.00

Hongkong, 17th June, 1905.

(33)

All communications intended for publication in "THE HONGKONG TELEGRAPH" should be addressed to The Editor, 1, Ice House Road, and should be accompanied by the Writer's Name and Address.

Ordinary business communications should be addressed to The Manager.

The Editor will not undertake to be responsible for any rejected MS., nor to return any Contribution.

SUBSCRIPTION RATES (IN ADVANCE).

DAILY—\$50 per annum.

WEEKLY—\$18 per annum.

The rate per quarter and per month, proportional, The daily issue is delivered free when the address is accessible to messenger. (2) Copies sent by post an additional \$1.00 per quarter is charged for postage. The postage on the weekly issue to any part of the world is 80 cents per quarter.

Single Copies, Daily, ten cents; Weekly, twenty-five cents.

BIRTHS.

On 6th June, at "Boat Accord," Duke Street, Penang, the wife of Captain K. OWEN, of a daughter.

On 7th June, at Penang, the wife of Chief Inspector W. J. FOLEY, F. M. S. Police, of a daughter.

DEATHS

On 5th inst. at Port Swettenham, of pneumonia, WILLIAM GEORGE CHRISTIE WALTER, aged 42.

On 7th June, at Ayer Itam, Penang, LEWIS EBERT, aged 74 years.

The Hongkong Telegraph

HONGKONG, SATURDAY, JUNE 17, 1905.

THE HARBOUR MASTER'S
REPORT.

The latest annual report on Hongkong, which covers the work of the harbour department, during 1904, shows that the Colony's advance towards even greater prosperity is being steadily maintained. The Hon. Capt. Barnes Lawrence remarks, in his report that, during the year reviewed, the tonnage entering and clearing, exclusive of steam launches in local trade amounted to 24,754,042 tons, being an increase, compared with 1903, of 714,180 tons, and the highest tonnage yet recorded. There were 58,933 arrivals of 12,388,892 tons, and 58,099 departures of 12,365,150 tons. Of British ocean vessels, 1,062 ships of 3,863,803 tons entered, and 2,136 ships of 3,845,932 tons cleared, while of river steamers under the British flag there were 2,072 ships of 2,849,895 tons entered and 2,031 ships of 2,847,461 tons cleared. Taken altogether, British ocean tonnage represented... 31.2% Foreign " " " 21.3% British river tonnage " 23.1% Foreign " " 1.9% Steam-launches in foreign trade represented " 5% Junks in foreign trade represented... 10.5% Junks in local trade " 9.5% 100.0%

7,435 steamers, 26 sailing vessels, 1,037 steam-launches, and 18,120 junks, in foreign trade, entered during the year, giving a daily average of 70, as against 68 in 1903.

For vessels of European construction, exclusive of steam-launches, the daily average was 20.36 against 19.9 in 1903.

Vessels under the British flag show an increase of 1,741 ships of 3,155,798 tons, of which 1,389 ships of 1,25,498 tons are river steamers. The remainder, 352 ships of 930,300 tons, are a net increase in British ocean vessels, due to a large influx of colliers and other tramp steamers in ballast, attracted to the Far East by the war.

The increase in river steamers is due to the fact that new vessels started to ply during the year.

In vessels under foreign flags, there is a decrease of 1,108 ships of 1,346,897 tons, of which 159 ships are river steamers. The river steam- tonnage, however, shows an increase of 16,008 tons, giving a net decrease in foreign ocean shipping of 1,149 ships of 1,910,889 tons.

This decrease is due to:—(i) The disappearance of all Russians and almost all Japanese ships from the run since the war began. (ii) A large number of small German and Norwegian vessels being transferred to the Japanese coast trade. (iii) A considerable decrease in numbers (but increase in tonnage) of vessels under U.S.A. colours.

The decrease in numbers of foreign river steamers is due to the smaller number of individual vessel plying, while the increased tonnage is accounted for by the larger size of those left.

The actual number of ships of European construction (exclusive of river steamers and steam-launches) entering during 1904 was 885, being 509 British, and 376 foreign.

These 885 ships entered 4,007 times, and gave a total tonnage of 6,437,10 tons. Thus, compared with 1903, 144 more ships entered 12 times less, and with a total tonnage decreased by 466,471 tons.

TRADE.

Once more it is necessary, says the Harbour Master, to call attention to the want of accuracy in the returns under this heading.

There being no Customs House in the Colony, it is impossible for accurate returns to be compiled, but the information given by the officers and agents of ships might with advantage be of a fuller and more detailed description.

It is true that I am empowered to call for copies of manifests of cargo imported, but, so long as the favourite and comprehensive term

"Case of Merchandise" continues to be employed, such manifest will not help us much,

while to deal with the various weights and measures of the cases, &c., as entered in the manifest, would require a very much larger staff than we now possess. As it is, the returns are compiled by the assistant Harbour Master, from information given by the officers of the ships, or, in some cases the agents. It frequently happens that the officer giving the information is ignorant of the existence on board of certain items which it is imperative should be reported, e.g., dangerous goods.

This being so, I do not consider it necessary

to say more on this subject than that, from these prefatory reports, it appears that:—

Import cargo has increased by 167,229 tons, or 4.2%

Export cargo has increased by 360,742 tons, or 16.0%

Transit cargo has increased by 276,976 tons, or 0.6%

Banker coal shipped decreased by 10,065 tons, or 1.6%

The total reported import trade of the port for 1904 amounted to 26,188 vessels of 11,205,516 tons, carrying 7,889,978 tons of cargo, of which 4,738,052 tons were discharged at Hongkong. This does not include the number, tonnage, or cargo of junks, or steam launches employed in local trade.

The total export trade from the port was represented by 16,639 vessels of 11,199,850 tons, carrying 3,436,746 tons of cargo, and shipping 670,812 tons of bunker coal.

During the year 1904, 14,932 vessels of European construction of 19,227,312 ton (net register), reported having carried 10,572,844 tons of cargo.

REVENUE.

The total revenue collected by the Harbour

department during the year was \$300,933.95, being an increase of \$15,645.53 on the previous year:—

1. Light dues, \$ 72,330.16

2. Licences and internal revenue, ... 62,418.00

3. Fees of court and office, ... 165,185.79

Total, \$300,933.95

STEAM LAUNCHES.

On the 31st December, there were 263 steam

launches employed in the harbour. Of these,

105 were licensed for the conveyance of pas-

senger, 134 were privately owned, 16 were

the property of the Colonial government, and 6 belonged to the Imperial Government in charge of military authorities.

EMIGRATION.

76,304 emigrants left Hongkong for various places during the year. Of these, 52,196 were carried by British ships and 23,203 by foreign ships; 149,195 were reported as having been brought to Hongkong from places to which they had emigrated, and of these, 121,715 were brought in British ships and 27,460 by foreign ships.

SUNDAY CARGO WORKING.

During the year, 108 permits were issued under the provisions of the Ordinance. Of these, 78 were not availed of owing to its being found unnecessary for the ship to work cargo on Sunday, and the fee paid for the permit was refunded in each case. The revenue collected under this heading was \$37,625; this was \$2,825 more than in 1903.

SEAMEN.

21,830 seamen were shipped and 25,291 dis-

charged at the Mercantile Marine Office and on board ships during the year.

NAVAL NOTES.

The British sloop *Cadmus* has left for Weihaiwei.

The *Iphigenia* will leave for Singapore early on Monday morning.

The dockyard authorities are making prepa-

rations to refit the *Diadem* as flagship on her arrival in the Colony.

M.S. *Andromeda* is expected to proceed to

Weihaiwei on Tuesday, and will convoy some

of the new destroyers to the northern base.

The Admiralty has ordered the twin-screw, shallow-draught steamship *Sandpiper*, Lieut.-Com. H. T. Allay, to be recommissioned for another term of river service on the China-Station. A new crew will be sent from England to relieve her present ship's company.

In conclusion of my researches into the

subject, continues Dr. Pearce, my observations

leads to the conclusion, that pathologically

there is little evidence of tubercular infection

through the alimentary tract. If tubercle

bacilli enter the system through this channel,

they never leave evidence of having done so,

in the intestine. My results bear this out thoroughly.

Several interesting cases of poisoning came

before his notice during the year. One or two

deaths from opium poisoning occur regularly

every year, but the number of these cases, he says, would appear to be gradually diminishing.

The bacteriological examination of a Japanese disinfectant called "Disinfectol" shows

that this preparation is highly valuable as a

germicide agent. It is more effective, but at

the same time, more expensive than Joyce's

Fluid. The preparation and distribution of

vaccine lymph has been successfully prosecuted

throughout the past year. The lymph has

given universal satisfaction. Its only fault is

its limited maintenance of virulence. Its

activity would appear to be limited to a period

varying from 6 weeks to 2 months. On com-

pletion of the Bacteriological Institute, expe-

riments will be made in regard to this question.

New apparatus has been ordered from home

and the lymph will be prepared according to

the methods adopted in the Government

Lymph Laboratories in London.

THE PUBLIC MORTUARY.

1,551 Post-mortem examinations were held

during the year 1904.

The gradual diminution in the number of

TELEGRAMS.

[Reuter's.]

Fatal Explosion on H.M.S.
"Magnificent."

LONDON, 15th June.—While H.M.S. *Magnificent*, was at gun practice off Taku, a six-inch shell exploded inside the gun. Eighteen, including the Gunnery Lieutenant, were wounded.

The cartridge of the shell in the *Magnificent's* gun missed fire, whereupon the breech opened and an explosion occurred simultaneously.

Lieutenant Stobart and three men, injured by the explosion, are dead.

The Assassination of M. Delyannis.

The death of M. Delyannis is regarded in Greece as a national calamity and is causing serious confusion; funds generally are lower and the House was closed till after the funeral in order to avert a panic.

The Morocco Conference.

Mr. Lowther, British Minister at Morocco, presented a categorical refusal from Great Britain on the 8th June to join an international conference. Count Tattenbach on the 9th June conveyed Germany's acceptance.

Later.

Peace Preliminaries.

An official despatch from Washington states that Washington has been selected for a conference of the Plenipotentiaries.

France.

M. Rovier has decided to retain for the present the Foreign portfolio.

Royal Marriage.

The marriage of the Crown Prince of Sweden and Princess Margaret of Connaught has taken place at St. George's Chapel, Windsor; the weather was magnificent.

[Prince Gustav, Adolph of Sweden and Norway notwithstanding—Norway, the bridegroom of the Princess Margaret of Connaught, is not the first member of his House who has been married in England. The King, his grandfather, when he was there, some twenty years ago, found out the virtues of Bournemouth; and the King's second son, Prince Oscar, as he was then, spent an early springtime at that agreeable watering-place in 1888. The consequence was that he fell in love with the charming Miss Munck, and the consequence of that, we are told, was a marriage between them, which took place at Bournemouth on the 15th of March, the bridegroom's mother being present. On this same day Prince Oscar made formal renunciation of his right of succession, and assumed the title of Prince Bernadotte, by which he has since been known. Bernadotte in love has ever been thoroughgoing. The original Bernadotte married Alice Clary, in spite of another suitor, who was no less a personage than the future Napoleon Emperor.]

THE COMING OF SANDOW.

In all probability Hongkong will shortly be having a visit from Sandow, the strong man, who is touring the East in the cause of physical culture. He was due at Penang on the 6th inst., and is booked to open at Singapore on the 10th inst. Mr. Sandow has with him a troupe of 20 pupils who assist him in his demonstrations.

These pupils are amateurs who have joined Mr. Sandow for a tour round the world to assist him in demonstrating his celebrated system of physical culture. In addition all appear nightly in his great Graeco-Roman Arena. Mr. Sandow has just finished a phenomenally successful tour of India, appearing at the various theatres during his tour. It has been found that the theatres in the several parts of India are not spacious enough for holding the audiences that crush and jam around the doors, nightly endeavouring to gain admission to see the World's Celebrated Strong Man; and to avoid this for the rest of his Eastern tour he has just had an enormous canvas theatre built. The performance presents that of an ancient Graeco-Roman arena, the pupils during the performance assisting and playing their parts of ROMAN GLADIATORS, IN OLYMPIAN GAMES, and all kinds of athletic sports. This company is probably the largest and most expensive that has ever visited the Orient and the performance given is one that appeals to every one. A contemporary, in speaking of Mr. Sandow's performances in Calcutta, said at the time that the reception accorded to Mr. Sandow was absolutely unparalleled in the records of Calcutta, and we opine that it must have more than satisfied Sandow himself of the absorbing interest which his visit to India aroused. It may truly be said that no show that has ever come to the East has proved such a big draw or roused an audience to such a pitch of enthusiasm as that of Mr. Sandow in Calcutta. Sandow says that he believes himself to be far stronger man today than he was some seven years ago. He is now 36. He says that a man's strength will go on increasing with careful treatment until he is 45, which is, as a rule, the limit of human development. Sandow's measurements at the present day are as follows:

Weight 14 stone 6 lbs, height 5 ft. 10 1/2 inches, neck 18 inches, chest 48 inches, chest expanded 62 inches, thigh 46 inches, knee 14 inches, calf 18 inches, ankle 8 1/2 inches, upper arm 19 1/2 inches, forearm 17 inches, wrist 7 1/2 inches.

SHIPPING accommodation at Antwerp is about to be extended at a cost of ten millions sterling, guaranteed by the Belgian Government, for the construction of a canal of five miles. The works will occupy ten years. When completed Antwerp will be the first port of the world.

A QUESTION THAT FAILED.

THE STORY OF A FAILURE TO REACH SABEDO.

The following article is written by the special correspondent of the *Kobe Herald*—
Sasebo is present hemispherically sealed to all foreigners except the very few favoured ones summoned there on Government business, and although I set out for the famous Naval Station last week with the full determination to get through, come what might, I had at last to abandon my mission altogether beyond hope. By sitting tight at Hakodate-junction, the nearest point which I was able to reach, I might perhaps have caught glimpses of the various important parades going to and fro, but the waiting would have been long and the results uncertain, so in the recognised span of life is limited to three score year and ten, I hastened on to Nagasaki.

My intention had been to go to Sasebo and try to get information from the Captain of the *Oldhamia*, whose vessel was captured by the Russians near Formosa, and who had been transferred to a Russian vessel which came to grief during the battle. My information was that the *Oldhamia's* captain had been taken to Sasebo with the Russians, and this I found to be quite correct. Also, I hoped to have a look at the two famous men who have almost monopolised the attention of the world during the past few weeks, and even—for journalists should not limit their ambitions—to have a talk with them. But neither Yogo nor Rojensky was to come within my ken, nor was I to see Japan's huge, grim ships and note the difference between the present time and the period when they excited my admiration in the magnificent grandeur of the naval review two years ago.

I went with the roar of a lion, heralding my approach with telegrams and telephone messages to the lords-high-everybody likely to have influence. I returned like a lamb. And the entire staff of police at Hakodate turned out to offer their sympathy, and possibly also to see that I did nothing which under martial law might be considered a sin.

So I went to Nagasaki. My ardours was considerably damped by this time, but neither the police nor my own Consul being able to help me, I reluctantly gave in and submitted to the inevitable. The man who gets to Sasebo first, I'll admit to be a better man than I am, but I can't help hoping that he will have to overcome all the opposition which I met myself.

THE RUSSIAN SAILORS AT NAGASAKI.

I found Nagasaki celebrating the nation's triumph on a scale which, considering its population, did it credit. Every now and again, there was the startling crack of some gigantic firework and a bright glare which took one's attention from the illuminated earth and water to the sky. But such things are of constant occurrence, and I didn't go to Kiushu to gaze at fireworks, lantern processions and similar commonplaces. I wanted something exclusive, something to keep the cables busy, and next morning I obtained at least some slight return for my trouble.

I discovered that fifty-two Russians were lodging in the two French hotels, so I went straight there, and on my way saw a few of them strolling around the streets. Big, healthy-looking fellows they were, too, with nothing about them to indicate that only a week before they had fought through one of the biggest naval battles in the history of the world. On arriving at the hotel, which are situated within an easy stone's throw of each other, I found several Russian seamen hovering around the door, near which some curio dealers were displaying their wares. They all looked in good health and condition, though they seemed rather subdued, and in no way suggested the rowdiness one had expected after the reports of their doings at the various ports at which they touched during the voyage. There were also present a couple of policemen, and a Japanese, who I learned was acting as interpreter. I may here remark that anyone who says that Kiushu is under-policed can never have travelled outside the port of Nagasaki. There is some savage tribe which we are told cannot count beyond a hundred. I do not belong to that particular brand of savagery myself, but my mathematical powers are also limited, and I cannot say how many police interviewed me in the course of my journeys. Let me add, though, that I am rather pleased than otherwise at having become so well-known to the police, for I found them all excellent fellows. In the particular instance now referred to, the police outside the hotel refused to allow me to address any of the Russians, so I had to force to go to the police station and obtain the consent of the Superintendent.

THE LATE KOREAN CHARGE D'AFFAIRES.

IN LONDON.

The late Korean Charge d'Affaires in London (writes a correspondent who knew him well) was a typical representative of the Hermit Kingdom. Of a naturally quiet and reserved almost shy—dempour, he was known but little outside diplomatic and official circles, but to those who visited him at the Legation at Earl's Court he was at all times extremely courteous. He had acted as Charge d'Affaires since the departure of the Minister, though he had but a slight knowledge of the English language.

He was but rarely seen in Downing-st., and then only when some special business took him there; but he attended most of the levees, and other Court functions. Since the outbreak of war in the Far East his position has been somewhat anomalous one, and he has been out of touch more or less with his Government. The Consul-General for Korea in London is Mr. Pritchard Morgan, a former Liberal member for Merthyr Tydfil.

Tragedies at the foreign embassies and legations in London are happily of rare occurrence. In 1898 a determined attack was made upon Count Arco Valler, the then First Secretary of the German Embassy in London. The diplomat was leaving the well-known corner mansion in Carlton House terrace when he was twice fired at by a miscreant. One of the bullets entered his back, whilst another struck an unfortunate policeman on point duty.

RUSSIA'S "SIEVER LINING."

THE DAWN OF REFORM.

PEASANTS AND THE LAND.

In the black sky of the Russian crisis there is a faint glimmer of the dawn. Imperial promises made in December last have an appearance of being kept. The Emperor has declared his willingness to grant the reforms, even if the people agitate. "But if they do, they will be punished," he is reported to have said. The Rescript on the subject of religions has made a favourable impression; some semblance of a Constitution seems to be shadowed forth in the deliberations of the State Council, and there are signs of a change of attitude towards the Press. The mountain of bureaucracy is in labour; it may produce a ridiculous mouse, but that is better than nothing. When it comes, this first dim outline of a Parliament—composed of two members from each class of the community: professional men, merchants, artisans, peasants, etc., appointed by the Zemstvos in the different provinces—will have the character of an advisory board preferring advice to the autocrat. Those are, it seems the Imperial intentions as to a Constitution. More hopeful, too, is the outlook for the Press. When the "Birjewia" Vedomosti offended the other day by publishing a scuffle in which the Emperor was attacked under the thin disguise of a character in the story, the journal was not proceeded against administratively, but legally, by court of law. Usually, when a newspaper oversteps the narrow limits allowed for discussion—so narrow that the question of rubber-tires was a taboo because the carriages of the court had them—the police adopt one of a series of methods. Either they cut off the advertisements and thus deprive the organ of revenue, or they forbid the street sale; or perhaps, as an extreme measure, they suspend publication for a certain period. It is by no means uncommon to be told, when one inquires for a paper: "Oh, that will appear in two months' time!"

In proceeding by legal means against the journal in question, the Government seems to have really initiated a new policy towards the Press.

In his famous Rescript of December, the Emperor laid it down that the Press should be dealt with under the ordinary laws. The language appears strange in the mouth of a European sovereign. Yet, to declare that the laws shall be followed is really to enunciate a new principle in Russia. Herein lies the trouble: the laws are not followed; they are a dead letter. In their place, reigns the police-made or Civil Service law. It has the significance of martial law; that is to say, of the absence of law and the substitution of arbitrary action. On paper, Russia has an excellent Constitution, with all the good qualities of paternal Government; but in practice it avails nothing except to accentuate the oppressive character of the actual administration.

OLD BELIEVERS AND JEWS.

Liberty of conscience has certainly received an ampler interpretation than might have been expected in the recent Rescript, granting to the Old Believers the practice of their faith. It is really a great step, and its beneficial character is acknowledged even by the Liberal Party in Russia. It is difficult to say exactly how many are affected, but I have heard it computed that they number twenty-five millions. Yesterday, a deputation of Old Believers tendered their homage to the Tsar at Tsarkoe Selo, and expressed their gratitude for the privilege. These adherents of the Old Faith have known the bitterness of persecution. Two hundred years ago, the Patriarch Nikon, the supreme head of the Church, perceiving that there were grave errors in the version in use of the religious books, made extensive alterations in the text for the purpose of bringing it into line with religious practice. He imposed his authority (which is that of the Pope of Rome without his infallibility) upon the clergy and the great mass of the faithful, but a certain section refused to accept the new version, and continued with extreme conservatism their allegiance to the old. Burning and decapitation had not the effect of weakening the resistance of the Old Believers; on the contrary, the movement grew. It is in process of years new prophet arose amongst the Old Believers, and the body was split into a multitude of sects, each holding fiercely to its own interpretation of the Word. Lately these dissentients have lived under a somewhat milder regime. Authority has shut its eyes to their nonconformity. The officials charged with their overlooking have accepted heretics not to pursue their investigations too far. Consequently the sectarians have lived, more or less peacefully, in a false atmosphere of orthodoxy, and practised their religion in secret, saving the Church dues, they have been unmolested. So that even before the last Rescript their position had become supportable. Yet the official indulgence now given to their belief is, of course, of value, and has resulted in the unsealing of ancient places of worship and the open practice of the religion by many who were nominally Orthodox for fear of the penalties attaching to opposition.

The lot of the Jews is not bettered, by the Imperial ukase. Alexander II., who is supposed to have been less fervently religious than Alexander III. and Nicholas II., granted permission for a synagogue in St. Petersburg, but there is no extention of that privilege under the present regime. Jews are still shut out from positions of place and influence in the services and in the professions. A Jew may now practise as a barrister with permission of the Minister of Justice, but I am assured that for the past fifteen years there has been none of the faith admitted to the St. Petersburg bar, his position being limited to the subordinate one of licensee, or of a student keeping his terms. Naturally, the removal of Jewish disabilities would constitute a reform even more striking than the recognition of the Old Believers. On the whole, one may say that the tendency of the moment is to treat Jews and all other opposing elements in the nation with greater leniency than for some time past. Liberals attribute this new spirit to the chastening influence of military reverses in the Far East and M. Witte's fear of the Terrorists. The Grand Duke Serge was a bitter enemy of Israel, permitting no Jews to live in Moscow. At St. Petersburg, however, and in other places, certain categories of Jewish residents are allowed—merchants and men following a trade.

THE PEASANT "ET MEUS REX."

One of the Easter gifts of the Tsar was the remission of debt weighing for years past upon the peasantry. The total of those debts is equivalent to a sum of seventy-five million rubles. But, seeing that the people could not possibly pay in any case, it is a gift more in form than in reality. Moreover, the ultimate liability is the people themselves, though nobles and rich landed proprietors.

The great opposing force to the carrying out of reforms is the force of inertia of that great uneducated mass of peasantry which, impossible to interest in the subtleties of politics, knows only one sensation, the hunger for land. To the peasant that is God and the Tsar and the land—that is all. The Tsar is the appointed ruler of the tiller of the earth, the "Little Father" of the peasant. "There should be no intermediate classes," says and thinks the peasant; "ego et meus rex" is sufficient for the whole world of Russia. Such a disposition of mind makes the rude husbandman a most powerful instrument for the party of reaction.

The peasant is not patriotic in the European sense. He acknowledges as a painful necessity the existence of the State; present visually in the person of the tax-gatherer; he supposes, in a blind way, that he must be protected from outside foes; but the land, always the land: that is the "ultimo Thule" of his aspirations and political creed.

The peasant is not patriotic in the European sense. He acknowledges as a painful necessity the existence of the State; present visually in the person of the tax-gatherer; he supposes, in a blind way, that he must be protected from outside foes; but the land, always the land: that is the "ultimo Thule" of his aspirations and political creed.

The peasant is not patriotic in the European sense. He acknowledges as a painful necessity the existence of the State; present visually in the person of the tax-gatherer; he supposes, in a blind way, that he must be protected from outside foes; but the land, always the land: that is the "ultimo Thule" of his aspirations and political creed.

The peasant is not patriotic in the European sense. He acknowledges as a painful necessity the existence of the State; present visually in the person of the tax-gatherer; he supposes, in a blind way, that he must be protected from outside foes; but the land, always the land: that is the "ultimo Thule" of his aspirations and political creed.

The peasant is not patriotic in the European sense. He acknowledges as a painful necessity the existence of the State; present visually in the person of the tax-gatherer; he supposes, in a blind way, that he must be protected from outside foes; but the land, always the land: that is the "ultimo Thule" of his aspirations and political creed.

The peasant is not patriotic in the European sense. He acknowledges as a painful necessity the existence of the State; present visually in the person of the tax-gatherer; he supposes, in a blind way, that he must be protected from outside foes; but the land, always the land: that is the "ultimo Thule" of his aspirations and political creed.

The peasant is not patriotic in the European sense. He acknowledges as a painful necessity the existence of the State; present visually in the person of the tax-gatherer; he supposes, in a blind way, that he must be protected from outside foes; but the land, always the land: that is the "ultimo Thule" of his aspirations and political creed.

The peasant is not patriotic in the European sense. He acknowledges as a painful necessity the existence of the State; present visually in the person of the tax-gatherer; he supposes, in a blind way, that he must be protected from outside foes; but the land, always the land: that is the "ultimo Thule" of his aspirations and political creed.

The peasant is not patriotic in the European sense. He acknowledges as a painful necessity the existence of the State; present visually in the person of the tax-gatherer; he supposes, in a blind way, that he must be protected from outside foes; but the land, always the land: that is the "ultimo Thule" of his aspirations and political creed.

The peasant is not patriotic in the European sense. He acknowledges as a painful necessity the existence of the State; present visually in the person of the tax-gatherer; he supposes, in a blind way, that he must be protected from outside foes; but the land, always the land: that is the "ultimo Thule" of his aspirations and political creed.

The peasant is not patriotic in the European sense. He acknowledges as a painful necessity the existence of the State; present visually in the person of the tax-gatherer; he supposes, in a blind way, that he must be protected from outside foes; but the land, always the land: that is the "ultimo Thule" of his aspirations and political creed.

The peasant is not patriotic in the European sense. He acknowledges as a painful necessity the existence of the State; present visually in the person of the tax-gatherer; he supposes, in a blind way, that he must be protected from outside foes; but the land, always the land: that is the "ultimo Thule" of his aspirations and political creed.

The peasant is not patriotic in the European sense. He acknowledges as a painful necessity the existence of the State; present visually in the person of the tax-gatherer; he supposes, in a blind way, that he must be protected from outside foes; but the land, always the land: that is the "ultimo Thule" of his aspirations and political creed.

The peasant is not patriotic in the European sense. He acknowledges as a painful necessity the existence of the State; present visually in the person of the tax-gatherer; he supposes, in a blind way, that he must be protected from outside foes; but the land, always the land: that is the "ultimo Thule" of his aspirations and political creed.

The peasant is not patriotic in the European sense. He acknowledges as a painful necessity the existence of the State; present visually in the person of the tax-gatherer; he supposes, in a blind way, that he must be protected from outside foes; but the land, always the land: that is the "ultimo Thule" of his aspirations and political creed.

The peasant is not patriotic in the European sense. He acknowledges as a painful necessity the existence of the State; present visually in the person of the tax-gatherer; he supposes, in a blind way, that he must be protected from outside foes; but the land, always the land: that is the "ultimo Thule" of his aspirations and political creed.

The peasant is not patriotic in the European sense. He acknowledges as a painful necessity the existence of the State; present visually in the person of the tax-gatherer; he supposes, in a blind way, that he must be protected from outside foes; but the land, always the land: that is the "ultimo Thule" of his aspirations and political creed.

The peasant is not patriotic in the European sense. He acknowledges as a painful necessity the existence of the State; present visually in the person of the tax-gatherer; he supposes, in a blind way, that he must be protected from outside foes; but the land, always the land: that is the "ultimo Thule" of his aspirations and political creed.

The peasant is not patriotic in the European sense. He acknowledges as a painful necessity the existence of the State; present visually in the person of the tax-gatherer; he supposes, in a blind way, that he must be protected from outside foes; but the land, always the land: that is the "ultimo Thule" of his aspirations and political creed.

The peasant is not patriotic in the European sense. He acknowledges as a painful necessity the existence of the State; present visually in the person of the tax-gatherer; he supposes, in a blind way, that he must be protected from outside foes; but the land, always the land: that is the "ultimo Thule" of his aspirations and political creed.

The peasant is not patriotic in the European sense. He acknowledges as a painful necessity the existence of the State; present visually in the person of the tax-gatherer; he supposes, in a blind way, that he must be protected from outside foes; but the land, always the land: that is the "ultimo Th

Shipping—Steamers.

OCEAN STEAMSHIP CO., LTD.
AND
CHINA MUTUAL STEAM NAV. CO., LTD.

JOINT SERVICES.

FORTNIGHTLY SAILINGS FOR LONDON AND CONTINENT.
MONTHLY SAILINGS FOR LIVERPOOL.TAKING CARGO ON THROUGH BILLS OF LADING FOR ALL EUROPEAN,
NORTH AND SOUTH AMERICAN, WEST AUSTRALIAN, JAVA
AND SUMATRA PORTS.

EUROPEAN SERVICE.

FROM	STEAMERS	TO SAIL	DUE
GLASGOW and LIVERPOOL	"AIAK"	23rd June	
GLASGOW and LIVERPOOL	"IDOMEANEUS"	30th June	
GLASGOW and LIVERPOOL	"STENTOR"	7th July	
GLASGOW and LIVERPOOL	"PATROCULUS"	14th July	
GLASGOW and LIVERPOOL	"KEEMUN"	16th July	
GLASGOW and LIVERPOOL	"PAKLING"	18th July	
GLASGOW and LIVERPOOL	"ACHILLES"	28th July	

HOMeward.

FOR	STEAMERS	TO SAIL	DUE
*GENOA, MARSEILLES & L'POOL	"DEUGALION"	20th June	
AMSTERDAM, LONDON & ANTWERP	"CALCHAS"	20th June	
LONDON, AMSTERDAM & ANTWERP	"HYSON"	4th July	
LONDON, AMSTERDAM & ANTWERP	"GLAUCUS"	18th July	
*GENOA, MARSEILLES & L'POOL	"TELEMACHUS"	20th July	
LONDON, AMSTERDAM & ANTWERP	"AJAX"	1st August	
LONDON, AMSTERDAM & ANTWERP	"IDOMEENEUS"	15th August	
*GENOA, MARSEILLES & L'POOL	"STENTOR"	20th August	

*Taking Cargo for Liverpool at London Rates.

TRANS-PACIFIC SERVICE.

OPERATING IN CONJUNCTION WITH
THE NORTHERN PACIFIC RAILROAD CO.
AND TAKING CARGO ON THROUGH BILLS OF LADING TO ALL
OVERLAND COMMON POINTS IN THE UNITED STATES
OF AMERICA AND CANADA.

EASTWARD.

FROM	STEAMERS	TO SAIL	DUE
VICTORIA, SEATTLE, TACOMA, and all PACIFIC COAST PORTS	"KEEMUN"	19th July	

For Freight, apply to

BUTTERFIELD & SWIRE,
AGENTS.

Hongkong, 15th June, 1905.

CHINA NAVIGATION CO. LIMITED.

FOR	STEAMERS	TO SAIL	DUE
CHEFOO	"CHINKIANG"	19th June	
SHANGHAI	"TIENSIN"	20th "	
MANILA	"TAMING"	20th "	
SWATOW, WEI-HAI-WEI, CHEFOO and TIENSIN	"CHIHLI"	21st "	
SHANGHAI	"SHAOHSING"	21st "	
YOKOHAMA and KOBE	"CHINGTU"	21st "	
CEBU and ILOILO	"KAIFONG"	22nd "	
MANILA, ZAMBANGA, PORT DAR-			
WIN, THURSDAY ISLAND, COOK-			
TOWN, CAIRNS, TOWNSVILLE, BRIS-			
BANE, SYDNEY and MELBOURNE	"CHINGTU"	20th July	

*The Attention of Passengers is directed to the Superior Accommodation offered by these steamers, which are fitted throughout with Electric Light. Unrivalled table. A duly qualified Surgeon is carried.

† Taking Cargo on through Bills of Lading to all Yangtze and Northern China Ports.

‡ Taking Cargo and Passengers at through Rates for all New Zealand and other Australian Ports.

N.H.—REDUCED SALOON FARES, SINGLE AND RETURN, TO MANILA AND

AUSTRALIAN PORTS. (SEE SPECIAL ADVERTISEMENT).

For Freight or Passage, apply to

BUTTERFIELD & SWIRE,
AGENTS.

Hongkong, 15th June, 1905.



HONGKONG—MANILA.

Highest Class, newest, fastest and most luxurious Steamers
between Hongkong and Manila.—Saloon, amidships—Electric
Light—Perfect Cuisine—Surgeon and Stewardess carried.
—All the most up-to-date arrangements for comfort of
Passengers.CHINA AND MANILA
STEAMSHIP COMPANY, LIMITED.

Steamship.	Tons.	Captain.	For	Sailing Dates.
ZAFIRO	1540	R. Redger	MANILA	SATURDAY, 24th June, at Noon.
RUBI	2520	A. H. Notley	"	SATURDAY, 1st July, at Noon.

For Freight or Passage, apply to

SHEWAN, TOMES & CO.,
GENERAL MANAGERS.

Hongkong, 17th June, 1905.



HONGKONG—NEW YORK.

AMERICAN ASIATIC
STEAMSHIP CO.

FOR NEW YORK via PORTS AND SUEZ CANAL.

(With Liberty to Call at the Malabar Coast).

Steamship "NORDPOL"..... THURSDAY, 22nd June.

"INDRAWADI"..... 25th July.

For Freight and further information, apply to

SHEWAN, TOMES & CO.,
General Agents.

Hongkong, 12th June, 1905.

BOO-CHEONG,
STATIONER AND PAPER MERCHANT,
No. 20, Pottinger Street.HAS always on hand all varieties of
Stationery, Printing and Note Papers,
Copying Presses, also Automatic Cyclostyle
and Etching Duplicator.

Hongkong, 23rd February, 1905.

TSANG FOO & CO.,
COAL MERCHANTS AND STEVEDORES,
48, DES Vaux ROAD.SHIPS Coaled from alongside at the shortest
notice, and with all possible despatch.
Prices Moderate. Telephone No. 329.

Hongkong, 1st October, 1904.

Shipping—Steamers.

THE HONGKONG, CANTON AND
MACAO STEAMBOAT COMPANY,
LIMITED.

CHEAP EXCURSIONS TO MACAO!

THE Steamship

"HONAM,"

2,163 tons,

Captain H. D. Jones, will make a special trip

EVERY SUNDAY TO MACAO AND BACK.

Hour of Departure:

From Hongkong at 9 A.M., arriving at Macao

about Noon.

From Macao from 4 P.M. to 7 P.M. to suit tide,

arriving at Hongkong about 3 hours after

departure.

FARES:

First Class, Single \$2..... Return \$4

Second Class, Single \$1..... Return \$2

Children under 12 half-price.

Tickets may be obtained at the Office of the

Company, 18, Bank Buildings, Queen's Road

Central (opposite the Hongkong Hotel), or on

board the Steamer.

No CHITS will be accepted, and Servants'

Passages must be paid for.

T. ARNOLD,

Secretary.

Hongkong, 15th May, 1905. [52]

REGULAR STEAMSHIP SERVICE

TO NEW YORK,

VIA PORTS AND SUEZ CANAL,

(With Liberty to Call at Malabar Coast).

PROPOSED SAILINGS FROM HONGKONG.

1905. About

"MONTROSE"..... 27th June, 1905.

"ST. HUGO"..... 15th July.

"SHIMOSA"..... to follow.

For Freight and further information, apply to

DODWELL & CO., LIMITED,

Agents.

Hongkong, 8th June, 1905. [52]

INDO-CHINA STEAM NAVIGATION CO., LTD.

(PROJECTED SAILINGS FROM HONGKONG.—SUBJECT TO ALTERATION).

For Steamship On

SINGAPORE, STRABAYA & SAMARANG HOPSANG TUESDAY, 10th June, 3 P.M.

MANILA YUEN-SANG FRIDAY, 23rd June, 3 P.M.

SHANGHAI HANG-SANG FRIDAY, 23rd June, 3 P.M.

SINGAPORE, PENANG & CALCUTTA, SUI-SANG SATURDAY, 24th June, Noon.

* These Steamers have superior accommodation for First-class Passengers, and are fitted throughout with Electric Light.

† Taking Cargo on through Bills of Lading to Chefoo, Tientsin, Newchwang and Yangtze Ports.

For Freight or Passage, apply to

JARDINE, MATHESON & CO.,

General Managers.

Hongkong, 17th June, 1905. [460]

PORTLAND & ASIATIC STEAMSHIP CO.

PROPOSED SAILINGS FROM HONGKONG, VIA SHANGHAI, INLAND
SEA OF JAPAN, MOJI, KOBE AND YOKOHAMA,

FOR

PORTLAND, OREGON,

OPERATING IN CONNECTION WITH

THE OREGON RAILROAD AND NAVIGATION COMPANY.

Steamship Tons Captain To Sail at Daylight on

"NICOMEDIA"..... 4,370...Wagner..... July 1st, 1905.

"NUMANTIA"..... 4,370...Brehmer..... July 16th, "

"ARABIA"..... 4,483...Metzenthin..... August 6th, "

"ARAGONIA"..... 5,198...Schuld..... August 26th, "

Through Bills of Lading issued to Pacific Coast Points and all Eastern, Canadian and United States Points. For through rates of Freight and further information, communicate with or apply to

12.

TRIPS TO CANTON AND MACAO.

THE Yukon Company's Splendid Steamer

"YING KING,"

1,088 tons, Registered.

Captain E. J. Page, will leave Hongkong for

Canton every MONDAY, WEDNESDAY and

FRIDAY EVENING, at 9:30 P.M., returning to Hongkong every TUESDAY,

THURSDAY and SATURDAY, about 5 P.M.

GERMANY IN THE PACIFIC.

Not long ago, says the Australian *Mining Standard*, Germany (or, rather, the *Jaluit Gesellschaft*), by the imposition of absurdly heavy licensing fees, succeeded in driving British trade out of the Marshall Islands. Its right to do so would have been "undoubted" (whatever one might think of the unneighbourliness of such action) had it not constituted a violation of treaty rights. The object was to stop competition, and that was stopping it with a vengeance. Such action is as outrageous as the clapping on of prohibitory duties, constituting an unwarrantable interference with the rights of trade. Now comes news that the British have been elbowing out of New Britain (the German "New Pomerania"); but the case is vastly different. It may be unpleasant for Australia to have Germany galling at her expense, but she has no legitimate ground of complaint. Her traders have been outbidden, and the business that they did has been cut away from them. This has not been done, as was the case with the *Jaluit Co.*, by unfair imposition of fees, but by a fair and square business agreement with those whose produce has provided the attraction for Australian shipping. Germany has scored, as in the Marshalls, by an arbitrary prohibition of trade on the part of others, but by offering the producers of Kaiser Wilhelm's Land (which we know by the name of "German New Guinea") and of the islands of the Bismarck Archipelago better terms than they could obtain from the Australian shipping companies who have hitherto done business there. The company that has achieved this great result, by securing from all the settlers (British, as well as German) an agreement giving them a five years' monopoly in the carriage of their produce to either Sydney or Hongkong, is the Norddeutscher Lloyd. It may be argued that the company starts with a heavy advantage over its Australian rivals, inasmuch as it is heavily subsidised by the German Government. The advantage so given is not to be denied; but while it may not mean fair competition as between company and company, it is perfectly fair as between country and country. If Germany choose to pay for the development of her trade, and the Australian Commonwealth prefer the risk of losing old markets to the expending of money for their retention, and for the gaining of new ones, Australia cannot complain if she be beaten in the race. If two companies are competing for certain business, and the one set fit to expend an amount of money that will mean working at a loss for a time with an eye to capturing the trade and reaping eventual profits, who shall say that its action is unfair? And, if it be right for an individual, a firm, a company, to act in such a way, how can it be wrong for a nation? Hence it must be admitted by those who are not blinded by national prejudice that the gain of Germany is one legitimately obtained. It is already too late for the agreement effected by the Norddeutscher Lloyd to be "upset," and it is clear that for the next five years the trade of the whole of the German possessions on the coast of New Guinea and among the islands of the adjacent archipelago will be in the hands of the company. The British planters in the localities in question have preferred hitherto, despite the obstacles placed in their way, to despatch their copra to Sydney by Australian-owned vessels. As they have joined with the German, French, and Scandinavian planters in accepting terms of the Norddeutscher Lloyd, it is evident that those terms were of so salutary a character as to quite outweigh sentiment. The company has undertaken to establish a regular and systematic inter-island steamship service, and the copra will be collected direct from the plantations or from their nearest ports. This will save the planters the expense, with which up to the present they have been saddled, of maintaining a small fleet of craft of their own, to convey their produce to intermediate ports for transhipment. What wonder, then, that they should embrace with readiness a business-like proposal made to them to facilitate and cheapen the disposal of their produce? It is to the credit of the Norddeutscher Lloyd that it should have seen and seized the opportunity thus afforded; it is not to the credit of such Australian firms as control Australian shipping to that corner of the globe that they should not have seen the chance, or that, seeing it, they should have let it slip. Australia has been beaten fairly and squarely, and it is just as well to acknowledge it.

CULLINAN'S LUCK.

THE KING OF DIAMONDS.

Tom Cullinan, the discoverer of the Premier Diamond Mine, with its record find, "the Cullinan Diamond," is now saying a recent "Morning Leader" in London, occupying a handsome suite at Claridge's. His fortunate gentleman, who has found wealth and immortality in a great mine, is a tall, spare man, with a soft, low voice, and a calm, resolute manner that the popular imagination hardly associates with the colonial gold and diamond seeker.

Naturally the first question which a "Leader" representative put to him during a short conversation on 20th April was to the future of the great diamond. But Mr. Cullinan could not say, nor could he estimate its value.

"You see," he said, "the value of a diamond usually rises as its size increases. But if you apply this method of calculation to the diamond you are speaking of—well, you run into millions."

SEVERAL KOH-NOORS.

Mr. Cullinan says the famous stone could yield several Kohl-Noors, and still be larger than any diamond in the world. He does not insinuate that there are other diamonds of the same size in the mine.

The story of the discovery of the Premier mine is an interesting one. In March, 1898, it is said, diamonds were reported to have been found East of Pretoria. "Jim" Pippin, known to every miner the Veldt to the Zambesi, is reported to have said of the site of the present Premier mine, "If you sit on the crest of the Southern African hills, and look out on the veld in front of you, you cannot help seeing another Kimberley."

However, the Rand magnates did not take this view of the possibilities. "They didn't find trees and bushes similar to those that grow around Kimberley," laughed Tom Cullinan.

The result was that they refused the price for the Premier site—some £25,000—which was asked by its Boer owner, Oom Joachim Prinsloo.

JUMP OF A HUNDRED PER CENT.

Then came the day, Oom Prinsloo now asked £2,000. The Rand magnates still hesitated. But Tom Cullinan acted. "I went, not on the trees and bushes theory," he explained, "but on practical results."

With the help of a few friends he raised the requisite sum. To-day the mine is valued by the Stock Exchange, says Mr. Cullinan, at over £6 millions, if you include the Government share of the profits in the basis of calculation. Even this valuation, he says, is only based on the present output, and does not take into account the ability of the company to increase its production.

MORE THAN THEY CAN DISPOSE OF.
The mine, says Mr. Cullinan, is capable of producing more diamonds than the company can dispose of, without affecting the price of diamonds. As to the latter consideration, Mr. Cullinan pointed out that the world is taking all the diamonds that are being produced, with a demand for more. The American demand was £8 millions more last year than the year before.

Mr. Cullinan declares that the life of the mine is "unlimited," a not unimportant matter, considering that the Transvaal Government appropriate 60 cent. of its profits.

Mr. Cullinan is 43 years of age. He was born near Fort Beaufort, Cape Colony, and went to the Transvaal in 1884.

I went through a hard school," he remarked to the "Leader" interviewer, "and had to shift for myself since the age of 17, when my father died. I formed many syndicates, with any result; and it was 12 years before I struck the Premier mine."

He now owns much property in Johannesburg, and has a beautiful home at Parktown. He also owns a farm, which he keeps as a hobby.

Shipping.

Arrivals.

Chingtu, Br. s.s. 2,260, J. McD. Howie, 17th June—Australia 9th May, Gen.—B. & S.

Arratoon Apam, Br. s.s. 2,351, E. Fey, 16th June—Singapore 11th June, Gen.—D. S. & Co., Ltd.

Zaida, Br. s.s. 3,204, C. Willis, 16th June—Singapore 11th June, Gen.—M. & Co.

Chihli, Br. s.s. 1,144, G. Hooker, 16th June—Chefoo and Swatow 6th June, Gen.—B. & S.

Providence, Nor. s.s. 900, C. Cornelissen, 16th June—Bangkok 10th June, Rice—Order.

Hero, Nor. s.s. 2,417, Sylvester, 16th June—Moj 10th June, Coal.—M. B. K.

Kweihsien, Br. s.s. 1,703, W. D. Brymer, 17th June—Shanghai via Amoy and Swatow 11th June, Gen.—B. & S.

Clearancourt at the Harbour Office.

Charles Hardwick, for Canton.

Fritscho, for Swatow.

Shun Lee, for West River.

San Cheong, for Canton.

Kwongting, for Canton.

Chawau, for West River.

Carl Diederichsen, for Hoitow.

Him, for Bangkok.

Abercrombie, for Shanghai.

Cresader, for Batavia.

Kweihsien, for Canton.

Wingchau, for Macao.

Emma Luyken, for Swatow.

Holm, for Shau-u-tsung.

Departures.

June 17.

Bengal, for Europe.

Rubi, for Manila.

Kalgan, for Shanghai.

Wotang, for Tientsin.

Fri, for Haiphong.

Hengsia, for Canton.

Calish, for Canton.

Ranca, for Singapore.

Emma Luyken, for Swatow.

Him, for Bangkok.

Passenger Arrivals.

Per Arratoon Apam, from Singapore—Mr. Mrs. and Miss Roachdale, Messrs. Hutchinson, Hintop, 48 Chinese, and 1 Japanese.

Per Chingtu, from Australia—Major Morley, Lieut. Butler and Hobbs, U.S.A., Mr. and Mrs. Fearon and child, Mrs. Kew, Misses Hill, Lakeman, Mrs. Pinches, Mrs. Holm, Mrs. Mauley, and 7 Chinese.

Passengers departed.

Per Bengal, for London from Yokohama—Misses M. Discon and A. Brancher, From Nagasaki—Capt. and Mrs. Grindelius, and Miss Hanson, Hintop, 48 Chinese, and 1 Japanese.

Per Chingtu, from Australia—Major Morley, Lieut. Butler and Hobbs, U.S.A., Mr. and Mrs. Fearon and child, Mrs. Kew, Misses Hill, Lakeman, Mrs. Pinches, Mrs. Holm, Mrs. Mauley, and 7 Chinese.

Passenger departed.

Per Bengal, for London from Yokohama—Misses M. Discon and A. Brancher, From Nagasaki—Capt. and Mrs. Grindelius, and Miss Hanson, Hintop, 48 Chinese, and 1 Japanese.

Per Chingtu, from Australia—Major Morley, Lieut. Butler and Hobbs, U.S.A., Mr. and Mrs. Fearon and child, Mrs. Kew, Misses Hill, Lakeman, Mrs. Pinches, Mrs. Holm, Mrs. Mauley, and 7 Chinese.

Passenger departed.

Per Bengal, for London from Yokohama—Misses M. Discon and A. Brancher, From Nagasaki—Capt. and Mrs. Grindelius, and Miss Hanson, Hintop, 48 Chinese, and 1 Japanese.

Per Chingtu, from Australia—Major Morley, Lieut. Butler and Hobbs, U.S.A., Mr. and Mrs. Fearon and child, Mrs. Kew, Misses Hill, Lakeman, Mrs. Pinches, Mrs. Holm, Mrs. Mauley, and 7 Chinese.

Passenger departed.

Per Bengal, for London from Yokohama—Misses M. Discon and A. Brancher, From Nagasaki—Capt. and Mrs. Grindelius, and Miss Hanson, Hintop, 48 Chinese, and 1 Japanese.

Per Chingtu, from Australia—Major Morley, Lieut. Butler and Hobbs, U.S.A., Mr. and Mrs. Fearon and child, Mrs. Kew, Misses Hill, Lakeman, Mrs. Pinches, Mrs. Holm, Mrs. Mauley, and 7 Chinese.

Passenger departed.

Per Bengal, for London from Yokohama—Misses M. Discon and A. Brancher, From Nagasaki—Capt. and Mrs. Grindelius, and Miss Hanson, Hintop, 48 Chinese, and 1 Japanese.

Per Chingtu, from Australia—Major Morley, Lieut. Butler and Hobbs, U.S.A., Mr. and Mrs. Fearon and child, Mrs. Kew, Misses Hill, Lakeman, Mrs. Pinches, Mrs. Holm, Mrs. Mauley, and 7 Chinese.

Passenger departed.

Per Bengal, for London from Yokohama—Misses M. Discon and A. Brancher, From Nagasaki—Capt. and Mrs. Grindelius, and Miss Hanson, Hintop, 48 Chinese, and 1 Japanese.

Per Chingtu, from Australia—Major Morley, Lieut. Butler and Hobbs, U.S.A., Mr. and Mrs. Fearon and child, Mrs. Kew, Misses Hill, Lakeman, Mrs. Pinches, Mrs. Holm, Mrs. Mauley, and 7 Chinese.

Passenger departed.

Per Bengal, for London from Yokohama—Misses M. Discon and A. Brancher, From Nagasaki—Capt. and Mrs. Grindelius, and Miss Hanson, Hintop, 48 Chinese, and 1 Japanese.

Per Chingtu, from Australia—Major Morley, Lieut. Butler and Hobbs, U.S.A., Mr. and Mrs. Fearon and child, Mrs. Kew, Misses Hill, Lakeman, Mrs. Pinches, Mrs. Holm, Mrs. Mauley, and 7 Chinese.

Passenger departed.

Per Bengal, for London from Yokohama—Misses M. Discon and A. Brancher, From Nagasaki—Capt. and Mrs. Grindelius, and Miss Hanson, Hintop, 48 Chinese, and 1 Japanese.

Per Chingtu, from Australia—Major Morley, Lieut. Butler and Hobbs, U.S.A., Mr. and Mrs. Fearon and child, Mrs. Kew, Misses Hill, Lakeman, Mrs. Pinches, Mrs. Holm, Mrs. Mauley, and 7 Chinese.

Passenger departed.

Per Bengal, for London from Yokohama—Misses M. Discon and A. Brancher, From Nagasaki—Capt. and Mrs. Grindelius, and Miss Hanson, Hintop, 48 Chinese, and 1 Japanese.

Per Chingtu, from Australia—Major Morley, Lieut. Butler and Hobbs, U.S.A., Mr. and Mrs. Fearon and child, Mrs. Kew, Misses Hill, Lakeman, Mrs. Pinches, Mrs. Holm, Mrs. Mauley, and 7 Chinese.

Passenger departed.

Per Bengal, for London from Yokohama—Misses M. Discon and A. Brancher, From Nagasaki—Capt. and Mrs. Grindelius, and Miss Hanson, Hintop, 48 Chinese, and 1 Japanese.

Per Chingtu, from Australia—Major Morley, Lieut. Butler and Hobbs, U.S.A., Mr. and Mrs. Fearon and child, Mrs. Kew, Misses Hill, Lakeman, Mrs. Pinches, Mrs. Holm, Mrs. Mauley, and 7 Chinese.

Passenger departed.

Per Bengal, for London from Yokohama—Misses M. Discon and A. Brancher, From Nagasaki—Capt. and Mrs. Grindelius, and Miss Hanson, Hintop, 48 Chinese, and 1 Japanese.

Per Chingtu, from Australia—Major Morley, Lieut. Butler and Hobbs, U.S.A., Mr. and Mrs. Fearon and child, Mrs. Kew, Misses Hill, Lakeman, Mrs. Pinches, Mrs. Holm, Mrs. Mauley, and 7 Chinese.

Passenger departed.

Per Bengal, for London from Yokohama—Misses M. Discon and A. Brancher, From Nagasaki—Capt. and Mrs. Grindelius, and Miss Hanson, Hintop, 48 Chinese, and 1 Japanese.

Per Chingtu, from Australia—Major Morley, Lieut. Butler and Hobbs, U.S.A., Mr. and Mrs. Fearon and child, Mrs. Kew, Misses Hill, Lakeman, Mrs. Pinches, Mrs. Holm, Mrs. Mauley, and 7 Chinese.

Passenger departed.

Per Bengal, for London from Yokohama—Misses M. Discon and A. Brancher, From Nagasaki—Capt. and Mrs. Grindelius, and Miss Hanson, Hintop, 48 Chinese, and 1 Japanese.

Per Chingtu, from Australia—Major Morley, Lieut. Butler and Hobbs, U.S.A., Mr. and Mrs. Fearon and child, Mrs. Kew, Misses Hill, Lakeman, Mrs. Pinches, Mrs. Holm, Mrs. Mauley, and 7 Chinese.

Passenger departed.

Per Bengal, for London from Yokohama—Misses M. Discon and A. Brancher, From Nagasaki—Capt. and Mrs. Grindelius, and Miss Hanson, Hintop, 48 Chinese, and 1 Japanese.

Per Chingtu, from Australia—Major Morley, Lieut. Butler and Hobbs, U.S.A., Mr. and Mrs. Fearon and child, Mrs. Kew, Misses Hill, Lakeman, Mrs. Pinches, Mrs. Holm, Mrs. Mauley, and 7 Chinese.

Passenger departed.

Per Bengal, for London from Yokohama—Misses M. Discon and A. Brancher, From Nagasaki—Capt. and Mrs. Grindelius, and Miss Hanson, Hintop, 48 Chinese, and 1 Japanese.

Per Chingtu, from Australia—Major Morley, Lieut. Butler and Hobbs, U.S.A., Mr. and Mrs. Fearon and child, Mrs. Kew, Misses Hill, Lakeman, Mrs. Pinches, Mrs. Holm, Mrs. Mauley, and 7 Chinese.

Passenger departed.</

Mails.

MESSAGERIES MARITIMES

FRENCH MAIL STEAMERS.

MM
STEAM FOR SAIGON,
SINGAPORE, BATAVIA,
COLOMBO, ADEN, EGYPT,
MARSEILLES, LONDON,
HAVRE, BORDEAUX,
MEDITERRANEAN AND BLACK SEA PORTS.

The S.S. "GALEDONIEN,"
Captain Gregory, will be despatched for MAR-
SEILLES on TUESDAY, the 27th June,
at 1 P.M.

Passage tickets and through Bills of Lading
issued for above ports.

Cargo also booked for principal places in
Europe.

Next sailings will be as follows:-

S.S. "OCEANIEN" 11th July.
S.S. "TOURANE" 25th July.
S.S. "TONKIN" 8th August.

G. DE CHAMPEAUX,

Agent.

Hongkong, 13th June, 1905.

THE PENINSULAR AND ORIENTAL
STEAM NAVIGATION COMPANY.STEAM FOR
RAITS, CEYLON, AUSTRALIA, INDIA,
DEN, EGYPT, MEDITERRANEAN
PORTS, PLYMOUTH AND
LONDON.

(Through Bills of Lading issued for BATAVIA,
PERSIAN GULF, CONTINENTAL, AMER-
ICAN and SOUTH AFRICAN PORTS.)

THE Steamship

"NUBIA;"

Captain F. J. Fox, carrying His Majesty's
Mails, will be despatched from this for
BOMBAY, on SATURDAY, the 1st July,
at Noon, taking Passengers and Cargo for
the above Ports in connection with the Company's
S.S. "Mongolia," 9,500 tons, from Colombo.
Passenger's accommodation in which vessel is
secured before departure from Hongkong.

Silk and Valuables, all Cargo for France
and Tea for London (under arrangement) will
be transhipped at Colombo into the Mail
steamer proceeding direct to Marseilles and
London; other Cargo for London, &c., will be
conveyed from Bombay by the R.M.S. "Arca-
dia," due in London on the 13th August.

Parcels will be received at this Office until 4
P.M. the day before sailing. The Contents and
Value of all Packages are required.

For further Particulars, apply to

L. S. LEWIS,

Acting Superintendent.

Hongkong, 17th June, 1905.

For Sale.

GREEN ISLAND CEMENT COMPANY,

LIMITED.

PORTLAND CEMENT.

\$4.50 per Case 375 lbs. net ex Factory.

\$2.70 per Bag 250 lbs. net ex Factory.

SHEWAN, TOMES & CO.,

General Managers.

Hongkong, 7th March, 1905.

[50]

TUBORG BEER.

A FIRST CLASS PILSENER BEER
guaranteed free from Salicylic Acid,
and any other Chemicals.PRICE \$10.50 per case of 48 bottles (quarts)
or 6 doz. pints.

Special Prices for Quantities.

Sole Agents —

SIEMSEN & CO.

Hongkong, 10th January, 1905.

[57]

FOR SALE.

INCANDESCENT GASOLINE LAMPS

OF ALL DESCRIPTIONS,
from the best makers.

INCANDESCENT MANTLES,

CHIMNEYS,

GLOBES,

SHADES, &c.,

for GASOLINE AND GAS

LAMPS

at the most moderate

prices.

Lamps fixed up for

Buyers free of charge.

Naphtha of the best

kind kept in stock.

TAI KWONG CO.

56, Lyndhurst Terrace.

Hongkong, 2nd May, 1905.

[54]

TO LET.

TO LET.

First Floor, No. 12, QUEEN'S ROAD,

CENTRAL.

Second Floor, Nos. 12 and 14, QUEEN'S

ROAD, CENTRAL.

Apply to —

S. BISNEY,

Hongkong Hotel.

Hongkong, 8th June, 1905.

[639]

TO LET.

WITH IMMEDIATE POSSESSION.

FOREST LODGE, Caine Road.

Apply to —

H. N. MODY.

Hongkong, 4th May, 1905.

[527]

TO LET.

NO. 12, KNUTSFORD TERRACE,

KOWLOON.

Apply to —

THE HONGKONG LAND INVEST-

MENT & AGENCY CO., LTD.

Hongkong, 4th May, 1905.

[537]

TO LET.

A BUILDING at CAUSEWAY BAY, at

present in occupation of the Steam

Laundry Co., Ltd.

No. 1, RIPPON TERRACE.

FLATS in MORETON TERRACE, facing

Polo Ground.

OFFICES in course of erection, CON-

NAUGHT ROAD (near BLAKE PIER).

GODOWNS: PRAYA EAST.

Apply to —

THE HONGKONG LAND INVEST-

MENT & AGENCY CO., LTD.

Hongkong, 30th March, 1905.

[66]

TO LET.

SEMI-DETACHED VILLAS, Two,

in Garden Road, near the Ferry, with Fine

Bright and Airy Rooms. GAS and ELECTRIC

BELLS laid on. Commanding fine view of the

Harbour.

Rents very moderate.

Apply to —

H. RUTTONJEE,

No. 5, D'Aguilar Street,

37 and 38, Elgin Road, Kowloon.

Hongkong, 5th June, 1905.

[67]

CHEAP FARES, EXCELLENT ACCOMMODATION,

ATTENDANCE AND CUISINE, ELECTRIC

LIGHT, DOCTOR AND STEWARDESSES.

The twin-screw a.s. "Shawmut" and "Tremont"

are fitted with very superior accommodation

for first and second class passengers. The

large size of these vessels ensures steadiness

at sea. Electric fan in each room.

Barber's shop and steam-laundry. Cargo

carried in cold storage.

For further information, apply to

DODWELL & CO., LIMITED,

General Agents.

Queen's Buildings,

Hongkong, 23rd May, 1905.

[68]

TO LET.

ACHEE & CO.

ESTABLISHED 1859.

FURNITURE,

GENERAL HOUSEHOLD

REQUISITES,

&c., &c., &c.

—

Telephone 256.

AMATEUR WORK Receives PROMPT and CAREFUL ATTENTION.

Hongkong, 16th May, 1905.

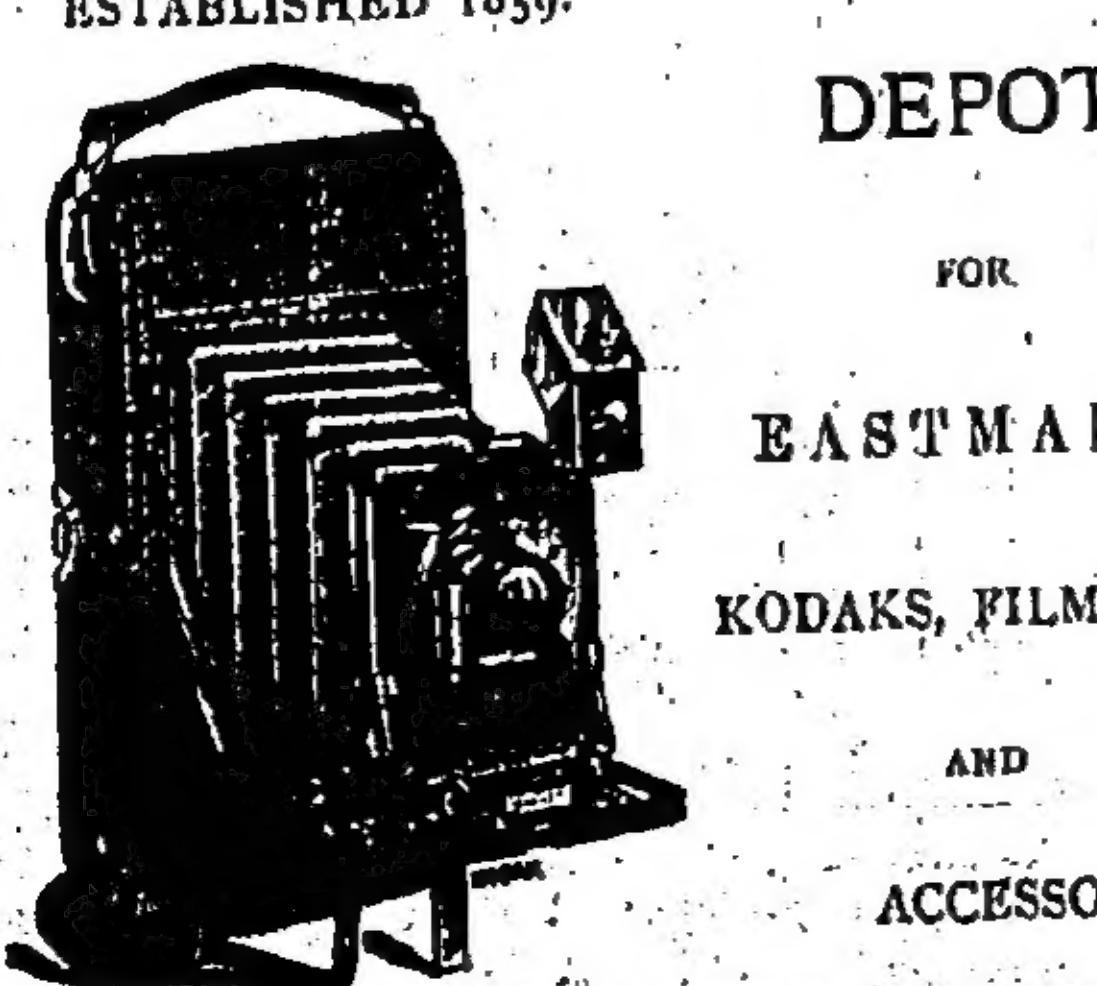
[69]

EASTMAN'S

KODAKS, FILMS,

AND

ACCESSORIES.



Telephone 256.

AMATEUR WORK Receives PROMPT and CAREFUL ATTENTION.

Hongkong, 16th May, 1905.

[70]

SHARE QUOTATIONS.

Supplied by Messrs. BENJAMIN, KELLY & POTTS. Connected to noon ; later alterations given in the "Commercial Intelligence," page 5.

STOCKS.	NO. OF SHARES.	VALUE.	PAID UP.	POSITION AS PER LAST REPORT RESERVE.	AT WORKING ACCOUNT.	LAST DIVIDEND.	APPROXIMATE RETURN AT PRESENT QUOTATION.	CLOSING QUOTATIONS.
HANKK.	80,000	\$125	\$125	{ \$1,000,000 \$80,000,000 \$250,000	\$1,493,408	{ Div. of \$1.10 and bonus of \$1 @ ex- change 1/10 9/10=\$2.46 for second half-year 1904	5 1/2 %	{ \$80 sellers London 83%
National Bank of China, Limited.	99,925	\$7	\$5	\$200,000	\$1,768	\$2 (London 3/6) for 1903	...	\$37 buyers
MARINE INSURANCES	10,000	\$250	\$50	{ \$1,400,000 \$1,759,359	\$10,494	\$17 for 1903	5 1/2 %	\$315 sellers
Tanton Insurance Office, Limited	10,000	\$125	\$5	Nil.	Nil.	\$4 1/2 for year ended 30.4.1904	7 1/2 %	\$65 buyers
China Traders' Insurance Company, Limited	24,000	\$83.33	\$25	Nil.	Nil.	Interim of 7/6 1904	8 %	Tls. 82
North China Insurance Company, Limited	10,000	\$125	\$5	Tls. 800,000	Tls. 217,119
Union Insurance Society of Canton, Limited	10,000	\$250	\$100	\$1,850,000	\$2,078,997	\$35 for 1903	5 %	\$660 buyers
Yangtze Insurance Association, Limited	8,000	\$100	\$60	\$77,749	Nil.
China Fire Insurance Company, Limited	20,000	\$100	\$20	\$700,000	\$86,284	\$12 and \$3 special dividend for 1903	8 1/2 %	\$172 buyers
Hongkong Fire Insurance Company, Limited	8,000	\$250	\$50	\$1,100,000	\$300,372	\$34 for 1903	11 1/2 %	\$302 sellers